



DEVELOPMENT SERVICES – PLANNING

8401 LAGUNA PALMS WAY • ELK GROVE, CALIFORNIA 95758

TEL: 916.683.7111 • FAX: 916.691.3175 • WWW.ELKGROVECITY.ORG

Revised NOTICE OF PREPARATION Elk Grove Crossing Specific Plan Draft Environmental Impact Report

Date: June 28th, 2024
To: Public Agencies and Interested Parties
From: City of Elk Grove (City)
Subject: Revised Notice of Preparation of a Draft Environmental Impact Report for the Elk Grove Crossing Specific Plan

This Notice of Preparation has been revised to add updated information related to planned infrastructure improvements and changes to the City’s General Plan that were not known at the time the previous Notice of Preparation (April 2, 2021) was released. There is no need to resend responses to the original Notice of Preparation. The City will use both sets of responses to inform the Environmental Impact Report.

The City will prepare a Draft Environmental Impact Report (EIR), compliant with the California Environmental Quality Act (CEQA), for the Elk Grove Crossing Specific Plan (also known as “the Project” and “the proposed Project”). The City has determined that an EIR will be prepared for the proposed Project, and an Initial Study will not be prepared (CEQA Guidelines Section 15063[a]). In accordance with Section 15082 of the CEQA Guidelines, this revised Notice of Preparation (NOP) provides a Project description, location, and the probable environmental effects of the proposed Specific Plan. This revised NOP will be circulated for a public review and comment period that begins on June 28th, 2024 and ends on July 29th, 2024.

The City is soliciting comments regarding the scope and content of the EIR related to other agencies’ statutory responsibilities in connection with the proposed Project, as well as comments from interested members of the public. The City will rely on responsible and trustee agencies to provide information relevant to the analysis of resources falling within the jurisdiction of such agencies. Specifically, input is required on:

- (1) Scope of Environmental Analysis – guidance on the scope of analysis for this EIR, including identification of specific issues that will require closer study due to the location, scale, and character of the Project;
- (2) Mitigation Measures – ideas for feasible mitigation, including mitigation that would avoid, eliminate, or reduce potentially significant or significant impacts; and
- (3) Alternatives – suggestions for alternatives to the proposed Project that could potentially reduce or avoid potentially significant or significant impacts.

The City welcomes public input during the review and comment period. If the City has not received a response (or a well-justified request for additional time) from a responsible or trustee agency by the end of the review

period, the City may presume that the responsible or trustee agency has no response (CEQA Guidelines Section 15082[b][2]). Please provide any written comments that you would like the City to consider, along with the name of the appropriate contact person in your agency and their contact information, to the address shown below by 5pm on July 29th, 2024.

City of Elk Grove Development Services Department
c/o Antonio Ablog
8401 Laguna Palms Way
Elk Grove, CA 95758
Email: aablog@elkgrovecity.org

PROJECT LOCATION AND SETTING

The Specific Plan Area is located approximately 1.7 miles west of the Kammerer Road/State Route (SR) 99 interchange, and south of Kammerer Road (see Exhibit 1). The Specific Plan Area encompasses approximately 319 acres of land area between the future extension of Big Horn Boulevard on the west and the future extension of Murphy's Corral Road on the east.¹ The Project site is within the Kammerer/Hwy 99 Sphere of Influence Area (SOIA), which was approved by the Sacramento Local Agency Formation Commission (LAFCo) in February of 2018.

The Specific Plan Area is surrounded by agricultural land to the north, northeast, and south, along with a Sacramento Metropolitan Utility District (SMUD) solar facility immediately adjacent to the southeast. The Kritter Kamp dog boarding and rescue facility is to the west, along with several 10-acre to 40-acre agricultural parcels and associated rural residences. Areas to the north of the Specific Plan Area are used for agriculture, but are zoned and anticipated for urban development under the Southeast Policy Area, the Livable Employment Area Community Plan, and the City's General Plan. Areas to the east are presently used for agriculture, but are within the SOI of the City of Elk Grove and within the City's South Study Area, where the City contemplates future urban development.

The City's General Plan envisions development of the South Study Area with office, retail, and commercial services, as well as mixed-use development, medium- and high-density residential development with transit access, and residential neighborhoods with easily accessible parks and open space. The City's Kammerer Road Urban Design Strategy recommended an approach to zoning along Kammerer Road that would allow transit-supportive, mixed-use development. The term used to embody this concept is "transect" zoning, and this concept is incorporated into the proposed Specific Plan for the northern portion of the Specific Plan Area. The City adopted a General Plan Amendment in 2023, which includes new transect-based land use designations. The City's Land Use Diagram was revised to include these new land use designations as part of the 2023 General Plan Amendment. Also included as part of the 2023 General Plan Amendment was a new Livable Employment Area Community Plan, which includes the Specific Plan Area and land to the east and north along Kammerer Road where the transect-based land use designations are focused. The Community Plan is a framework to provide for higher densities and intensities of development compared to other areas of the city – densities and intensities that are required to fulfill the vision and objectives of the Community Plan.

¹ Please see Exhibit 3-6, Transportation Network Diagram, in the City's General Plan, page 3-32 for more detail on the planned transportation network in the vicinity of the proposed Specific Plan Area:
http://www.elkgrovecity.org/UserFiles/Servers/Server_109585/File/Departments/Planning/Projects/General%20Plan/GPU/Amend_2019-12/Chapter%203_RevDec2019_web.pdf

PROJECT DESCRIPTION

The Elk Grove Crossing Specific Plan proposes a mix of uses including low-, medium-, and high-density residential, mixed-use development, parks and open space, and public uses (see Exhibit 2). The Specific Plan proposes higher densities and development intensities (e.g., retail, office, and high-density residential) in the northern portion of the Specific Plan Area along Kammerer Road, and lower intensity uses (e.g., single-family residential neighborhoods) in the southern portion of the Specific Plan Area.

Proposed Land Uses

The Specific Plan proposes mixed-use zoning districts in the northern portion of the Specific Plan Area adjacent to Kammerer Road, including Neighborhood Center High (T-5), Neighborhood Center Medium (T4), and Neighborhood Center Low (T3). The T5, T4, and T3 (“T” for “transect”) zoning designations allow for a mix of uses, densities, and development intensities to provide housing, employment, and commercial opportunities (Exhibit 2). The City’s General Plan includes guidance for the transect zoning areas along Kammerer Road related to land use, building placement, circulation, and other topics. Regarding the overall development pattern, the City requires that block perimeters are no more than 3,000 feet within the T3 zone, 2,500 feet in the T4 zone, and 2,000 feet in T5 zones.

Neighborhood Center High (T5) allows a mix of non-residential uses, including retail, commercial services, and lodging, as well as multi-family residential development and live-work spaces with residential densities between 30 and 100 units per acre and a maximum floor area ratio (FAR) for non-residential and mixed-use projects of up to 7. Neighborhood Center Medium (T4) allows a mix of non-residential uses, including retail, commercial services, lodging, and offices, as well as townhomes, apartment buildings, and other housing types with residential densities between 20 and 40 units per acre and a maximum floor area ratio (FAR) for non-residential and mixed-use projects of up to 5. Neighborhood Center Low (T3) allows a mix of non-residential uses, including local serving retail and commercial services and offices, as well as a mix of small-lot single-family residences, duplexes, townhomes, and small apartment buildings with residential densities between 14 and 30 units per acre and a maximum floor area ratio (FAR) for non-residential and mixed-use projects of up to 2.

There are two T3 zoned areas of 7.4 and 7.7 acres (shown as T3-6 and T3-7 on Exhibit 2) that have adjacent proposed park spaces of 1.6 acres and an adjacent area proposed for a sewer lift station. Medium-density residential areas totaling approximately 44 acres are anticipated to be developed at densities between 7.1 and 15 units per acre.

Two park sites of approximately 9.8 acres and 6.6 acres each are proposed in the central and southern portions of the Specific Plan Area. The larger park is proposed adjacent to a proposed 10.4-acre site reserved for an elementary school, providing an opportunity for the shared use of facilities. South of each of the parks are flood control and water quality basins for stormwater management, of 10 and 5 acres in land area. The remainder of the Specific Plan Area, a total of approximately 122 acres, is proposed for low-density residential development, which would range in density from 4.1 units per acre to 7 units per acre.

Along the southern Specific Plan Area boundary, an existing irrigation/drainage channel conveys stormwater approximately four miles to the west, where it crosses under Interstate 5 and connects to Stone Lake. Portions of this channel along the Specific Plan Area boundary would be realigned to be located within an 80-foot-wide open space parcel. A bicycle/pedestrian trail would be developed along the north and westerly side of this channel. The

drainage channel and associated trail would also provide a buffer between the development proposed within the Specific Plan Area and ongoing agricultural activities that may continue to the south.

Utilities and Service Systems

The proposed Project will require on- and off-site installation of supporting underground utilities, including water, wastewater, electricity, natural gas, and telecommunications (Exhibit 3). The Specific Plan will include detailed descriptions of the planned infrastructure improvements that will be required to serve demand resulting from development within the Specific Plan Area. Infrastructure for the Specific Plan will be required to comply with relevant design standards of service agencies and will be required to be consistent with the public facilities and infrastructure policies of the City's General Plan.

DRAINAGE

As detailed in the Drainage Study for Elk Grove Crossing, the Specific Plan will be required to ensure that post-development drainage flows do not exceed predevelopment conditions. The Specific Plan proposes the use of flood control basins that detain and discharge flows into the existing irrigation ditch along the southern portion of the site. Two detention basins to both treat stormwater runoff and provide stormwater metering are proposed. The Specific Plan will be required to be consistent with Section 9 of the City of Elk Grove Improvement Standards, which in turn are consistent with Volume 2 of the Sacramento City/County Drainage Manual. The Specific Plan is required to demonstrate consistency with the Sacramento County Stormwater Quality Partnerships Hydromodification Management Plan for configuring the detention basins with hydromodification outlets to mitigate post development conditions for the on-site watersheds.

WATER SUPPLY

The Sacramento County Water Agency (SCWA) Zone 40 service area currently ends at Kammerer Road, at the northern edge of the Specific Plan Area. The City proposes annexation of the Specific Plan Area by SCWA, as the Specific Plan Area is outside of SCWA's South Service Area. The South Service Area is supplied mainly by surface water and groundwater. The surface water comes from the Franklin intertie and the Vineyard Surface Water Treatment Plant, and the groundwater is produced from the existing groundwater treatment plants (GWTP) (i.e., Lakeside GWTP, Poppy Ridge GWTP, and Big Horn GWTP) and several direct-feed wells (where the pumped water is not processed through a treatment plant). The Specific Plan will also require approval by the State Water Resources Control Board of a change in the change in the geographic location of delivery of water (the place of use).

The Specific Plan Water Study included development of a hydraulic model for the on-site backbone water system that would be necessary to serve the Specific Plan Area and to determine the required size of the proposed facilities based upon SCWA's operating criteria. The proposed Specific Plan Area would tie into existing SCWA South Service Area infrastructure via two off-site points of connection: an existing 20-inch transmission main at the corner of Bilby Road and Big Horn Boulevard, and an existing 20-inch transmission main in Kammerer Road (Exhibit 4). Irrigation systems within the Specific Plan Area would be designed so that they can be converted to recycled water once recycled water if recycled water mains become operational.

SEWER

The Specific Plan Area is within the Sacramento Area Sewer District (SacSewer) Sphere of Influence. A Level II Sewer Study has been prepared for the Specific Plan that provides details regarding on-site and off-site wastewater collection and treatment (Exhibits 5 and 6). The Specific Plan would require the construction and operation of an on-site collection system, an on-site pump station, and two parallel off-site force mains. The Specific Plan's on-site backbone sewer collection system would extend south within a roadway along the central green space to serve the residential neighborhoods in the southern portion of the Specific Plan Area, and north along Ezekiel Drive and Merritt Lane to serve the northern portion of the Specific Plan Area. Wastewater generated by development within the Specific Plan Area would flow via gravity to a pump station located on the north side of Bear Flag Drive. From this pump station, wastewater would be conveyed via two parallel 12-inch and 16-inch force mains west along Bear Flag Drive to the western edge of the Specific Plan Area, and then north to Kammerer Road. The two parallel force mains would continue northward off the Specific Plan Area to the north and west for a distance of approximately five miles. From Kammerer Road, the underground force mains would be installed within the road rights-of-way in Big Horn Boulevard to its intersection with Bond Road (Laguna Boulevard), at which point the alignment would turn west on Bond Road (Laguna Boulevard) to connect with the force main/discharge associated with the existing Laguna Interceptor.

TRANSPORTATION

In addition to the future construction of Big Horn Boulevard to the west, Murphy's Corral Road to the East, and the widening of Kammerer Road to the north (part of the Capitol Southeast Connector), the Specific Plan Area would include an internal roadway circulation network consisting of arterials and collector streets. All internal roadways would be landscaped with trees and shrubs (Exhibit 7).

The Specific Plan proposes a network of bikeways and walkways to serve future residents, visitors, and employees (Exhibit 8). A 10-foot, Class I Trail is proposed through the center of the Specific Plan Area and along the southern and southeastern boundary. On the west side of the school site an eight-foot walk is proposed. On collector streets, a separated six-foot walk is proposed. On arterial streets, a separated six-foot walk is proposed. Along Kammerer Road, a 12-foot multi-use trail is proposed.

REQUIRED APPROVALS

In February of 2018, the Sacramento Local Agency Formation Commission (LAFCo) approved a landowner-initiated proposal to amend the City of Elk Grove Sphere of Influence (SOI), the SASD SOI, and the Regional San SOI. This SOI amendment area comprises 1,156 acres of land area that abuts the southern portion of the City's existing jurisdictional boundary and includes the entire proposed Specific Plan Area. After the LAFCo approval of the SOI amendment, the City updated the General Plan, which included the proposed Specific Plan Area as a part of the South Study Area. As noted previously, another General Plan amendment added the mixed-use, transit-supportive transect zoning concepts, with policy guidance for the Specific Plan and other proposed developments along Kammerer Road.

The Project includes a request for the approval of a Specific Plan and rezoning for future annexation of the Specific Plan Area into the City limits. Approval and buildout of the Specific Plan (including subsequent developments proposed under the Specific Plan) is anticipated to require approvals and actions which may include, but is not necessarily limited to:

- City of Elk Grove certification of the EIR and adoption of the Mitigation Monitoring and Reporting Program; adoption of the Elk Grove Crossing Specific Plan; rezoning; approval of a large-lot tentative map; and approval a development agreement;
- Future projects within the Specific Plan Area may require additional subsequent entitlements from the City, including small-lot tentative subdivision maps, design review, and/or conditional use permits and will require issuance of grading and building permits;
- U.S. Army Corps of Engineers Section 404 permit;
- Central Valley Regional Water Quality Control Board and State Water Resources Control Board 401 certification or water discharge permit;
- California Department of Water Resources, potentially to authorize a change in the Place of Use Boundary related to placement of the Specific Plan Area within the Zone 40 service area of the Sacramento County Water Agency;
- Sacramento Metropolitan Air Quality Management District approval of an operational air quality mitigation plan;
- Sacramento County Water Agency approval to include the Specific Plan Area within the Zone 40 service area and approval of proposed water supply improvements;
- Sacramento Area Sewer District approval of proposed wastewater collection and wastewater treatment;
- Cosumnes Community Services District for parks and recreational facilities and fire suppression services;
- Sacramento Metropolitan Utility District approval of electrical facilities; and
- Sacramento LAFCo approval of annexation of the Specific Plan Area.

Other local, State, or federal approvals or permits may be necessary for subsequent land use entitlements, pursuant to applicable laws and regulations.

APPROACH TO ENVIRONMENTAL REVIEW

As required by CEQA, the EIR will describe existing conditions and evaluate the potential significant environmental effects of the proposed Elk Grove Crossing Specific Plan and a reasonable range of alternatives, including a no project alternative. The EIR will address direct, reasonably foreseeable indirect, cumulative, and growth inducing effects. The EIR will identify feasible mitigation measures, if available, to reduce significant and potentially significant impacts.

POTENTIAL ENVIRONMENTAL EFFECTS

The following environmental topic areas are preliminarily anticipated to be evaluated in the EIR:

- **Aesthetics**—The EIR will evaluate existing visual conditions and will evaluate the potential impacts on scenic vistas, scenic resources, and visual character that may result from development of the Specific Plan Area and off-site improvement areas, as well as impacts related to light and glare.
- **Agriculture and Forestry Resources**—The EIR will describe existing agricultural resources and evaluate potential direct and indirect impacts from conflicts with existing zoning and from conversion to urban uses of lands designated as Prime Farmland, Unique Farmland, or Farmland of Statewide Importance and any lands within a Farmland Security Zone (as well as convey information presented in the previously certified Kammerer/Hwy 99 SOIA EIR related to the Cortese Knox Act definition of prime farmland). This section will also document that no forest land resources are present. Williamson Act contracts for the Specific Plan Area have expired.
- **Air Quality**—The EIR will discuss the regional and local air quality setting and quantify air pollutant emissions for construction and subsequent operation based on the proposed land use designations and assumptions related to off-site improvement areas. Emissions will use methods consistent with, and will be compared with the significance thresholds developed by the Sacramento Metropolitan Air Quality Management District (SMAQMD), and the EIR will explain how analytical methods and thresholds are designed to assess potential human health effects.
- **Biological Resources**—The EIR will define the biological resources in the vicinity of the Specific Plan Area and off-site improvement areas and surrounding habitats and evaluate the Specific Plan’s potential effects on wetlands, other sensitive natural communities, and special-status species (e.g., raptors and other migratory birds). This section will also address the relationship between the proposed Specific Plan and the *South Sacramento Habitat Conservation Plan*.
- **Cultural and Tribal Cultural Resources**—The EIR will describe existing cultural and tribal cultural resources and evaluate potential impacts on those resources, including the potential to affect undiscovered resources during excavation and grading. The EIR will also include consultation with California Native American tribes to assess potential impacts on tribal cultural resources.
- **Energy**—The EIR will describe current electricity and natural gas utility providers and evaluate whether the Specific Plan would have any potentially significant effects related to energy demand, energy resources, transportation energy use, or compliance with energy standards.
- **Geology, Soils, Minerals, and Paleontology**—The EIR will describe the geological setting and potential environmental effects related to geologic and soils hazards, mineral resources, and unique paleontological (fossil) resources. This section will outline design measures and best management practices to minimize impacts on people or structures from seismic activity. The EIR will also identify any potential impacts from loss of mineral resources and on undiscovered fossils.
- **Greenhouse Gas Emissions**—The Specific Plan will be required to demonstrate consistency with the City’s Climate Action Plan, and the EIR will document this consistency for the purposes of evaluating GHG emissions effects consistent with CEQA Guidelines Section 15183.5.
- **Hazards, Hazardous Materials, and Wildfire**—The EIR will identify potential impacts from the transport, use, or disposal of hazardous materials; releases of hazardous materials; emissions of hazardous or acutely

hazardous materials, substances, or waste near a school; location on a hazardous materials site; location within an airport land use plan or in the vicinity of a private airstrip; impairment of an adopted emergency response or evacuation plan; and exposure to wildland fires.

- **Hydrology and Water Quality**—The EIR will evaluate hydrologic and water quality conditions and potential short-term construction-related effects on water quality from stormwater runoff, as well as longer term effects on stormwater drainage and maintenance effects on water quality. This section will also evaluate potential impacts on groundwater recharge and sustainability, and on surface water hydrology from the addition of impervious surfaces associated with future residential, institutional, commercial, and industrial development. This section will outline the design features and stormwater retention features required to minimize hydrology and water quality effects, as well as the proposed Specific Plan’s consistency with regional flood protection planning.
- **Land Use and Planning, Population, and Housing**—The EIR will describe existing land uses and evaluate the potential for the proposed Specific Plan to divide an existing community or conflict with existing, adopted land use and natural resource plans or regulations that were adopted with the purpose of reducing or avoiding environmental effects. The EIR will also convey information from the previously certified Kammerer/Hwy 99 SOIA EIR related to the loss of open space resources (as defined by Government Code Section 65560). The EIR will evaluate the potential of the proposed Specific Plan or off-site infrastructure improvements to induce substantial population growth or displace substantial numbers of housing units or people that could lead to potentially significant physical environmental effects.
- **Noise and Vibration**—The EIR will describe existing noise and vibration conditions and the potential impacts of construction of the proposed land uses and off-site improvements. Noise levels from construction and operation of proposed uses will be estimated, accounting for intervening topography, noise barriers, and distance, and will be compared with existing ambient noise levels and applicable noise standards.
- **Public Services and Recreation**—The EIR will analyze existing public services and potential increases in demand, and will evaluate whether those demands would require new facilities (e.g., schools, fire protection, and law enforcement) that could result in potentially significant environmental impacts. The EIR will also evaluate impacts related to recreational facilities, including indirect effects on existing facilities.
- **Transportation**—The EIR will identify existing conditions, existing plus Project conditions, cumulative no-Project conditions, and cumulative plus Project conditions. The EIR will present a level of traffic stress analysis for pedestrian and bicycle facilities in the vicinity of the Specific Plan Area to characterize the comfort of streets and intersections for pedestrians and the mental ease people experience cycling. The EIR will summarize an analysis of vehicular travel demand (vehicle miles traveled or “VMT”) using the significance thresholds presented in the City’s General Plan. The EIR will evaluate hazards due to a design feature or incompatible use; inadequate emergency access; and conflicts with adopted plans, policies, or programs regarding public transit, bicycle, or pedestrian facilities.
- **Utilities**—The EIR will evaluate physical environmental impacts related to the on- and off-site expansion and extension of utility systems, such as those that are required for water supply, stormwater drainage, wastewater collection and treatment, and solid waste disposal. As noted, infrastructure that will serve the Specific Plan

will need to be provided consistent with relevant service agency standards and consistent with the City's General Plan.

LOCAL AGENCY FORMATION COMMISSION

Analysis and documentation prepared to support the Specific Plan will also address topics that are the purview of the Sacramento Local Agency Formation Commission (LAFCo), including a Plan for Services that will identify service providers and whether they can provide service without adversely affecting existing service levels, provision of service without adverse impact to existing ratepayers, and whether the City would perform any services now being provided by another service provider, and whether substitution of the City for that provider would have adverse effects of the previous providers' ability to maintain services. The City anticipates that material to support the Specific Plan would also address environmental justice, consistent with City policy. The Specific Plan would not require any detachment from a special district and the City's Housing Element does not identify any sites in the Specific Plan Area intended for the provision of housing to meet the needs of very low-, low-, or moderate-income households.

Exhibit 1. Regional Location Map

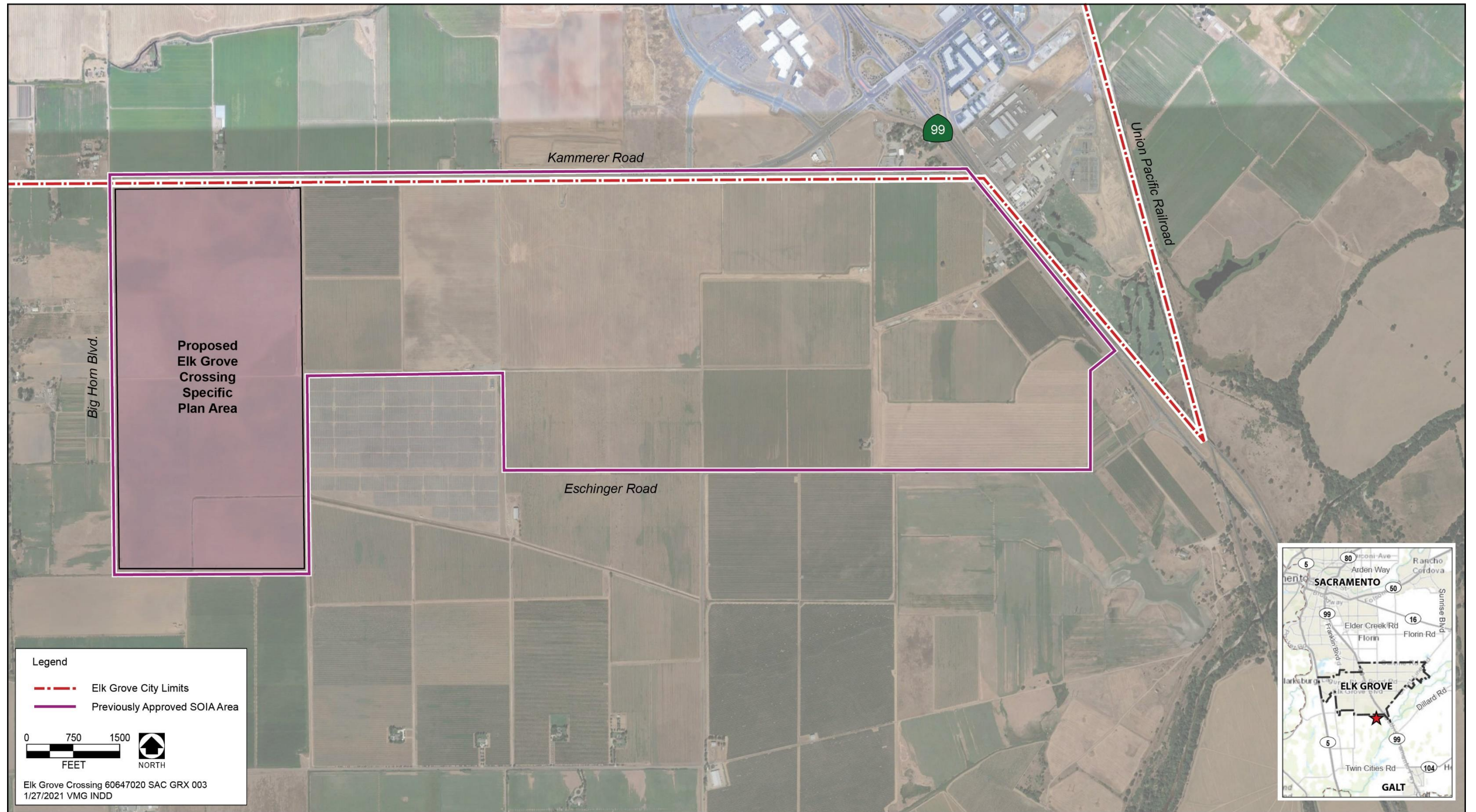
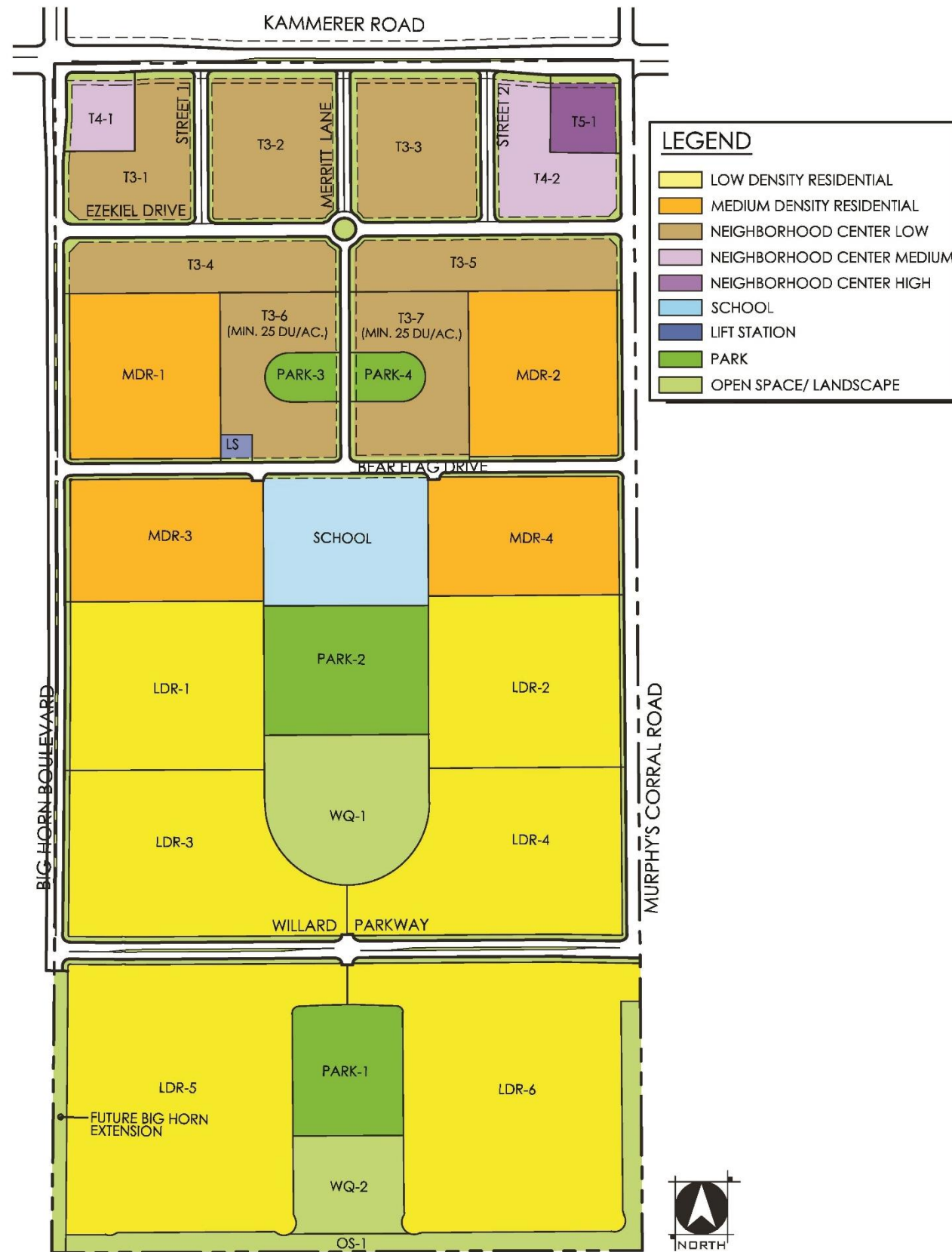


Exhibit 2. Conceptual Land Use Plan and Land Use Summary Table



Above: Proposed Land Use Plan

Proposed Land Use Summary Table

Specific Plan				
Block ID	Land Use	General Plan Land Use Designation	Implementing Zoning District	Block Net Acreage (±)
LDR-1	Low Density Residential	Low Density Residential (4.1-7.0 du/ac)	RD-6, RD-7	15.0
LDR-2				15.2
LDR-3			RD-5, RD-6	17.6
LDR-4				17.4
LDR-5			RD-4, RD-5	28.9
LDR-6				28.5
Subtotal LDR				122.6
MDR-1	Medium Density Residential	Medium Density Residential (7.1-15.0 du/ac)	RD-8, RD-10, RD-12, RD-15	11.5
MDR-2				11.3
MDR-3			10.9	
MDR-4			10.4	
Subtotal MDR				44.1
T3-1	Neighborhood Center Low	Neighborhood Center Low (14.0-30.0 du/ac or Maximum FAR of 2.0)	LEASPA-T-3	7.1
T3-2				10.0
T3-3				9.9
T3-4				7.1
T3-5				7.0
T3-6	Neighborhood Center Low (Minimum 25 du/ac)		LEASPA-T-3 (Minimum 25 du/ac)	7.4
T3-7				7.7
Subtotal T3				56.2
T4-1	Neighborhood Center Medium	Neighborhood Center Medium (20.0-40.0 du/ac or Maximum FAR of 5.0)	LEASPA-T-4	2.5
T4-2				6.6
Subtotal T4				9.1
T-5	Neighborhood Center High	Neighborhood Center High (30.0-100.0 du/ac or Maximum FAR of 7.0)	LEASPA-T-5	2.4
Subtotal T5				2.4
P/QP	Elementary School	Public Services (PS)	PS (RD-7)	10.4
Subtotal School				10.4
LS	Lift Station	Public Services (PS)	PS (RD-8, 10, 12, 15, or 30)	0.4
Subtotal Lift Station				0.4
Parks-1,2	Neighborhood Park	Parks/Open Space	PR	16.4
Parks-3,4	Local Park	Parks/Open Space	PR	3.2
OS-1	Open Space/Buffer	Resource Management & Conservation (RMC)	O	7.1
WQ-1	WQ/Detention Basin	Resource Management & Conservation (RMC)	O	10.0
WQ-2				5.0
Subtotal Park/OS				41.7
Landscape Corridors				7.0
Big Horn ROW Set-Aside				1.8
Major Roads				23.6
TOTAL				318.9

Exhibit 3. Specific Plan Area and Off-Site Improvement Areas

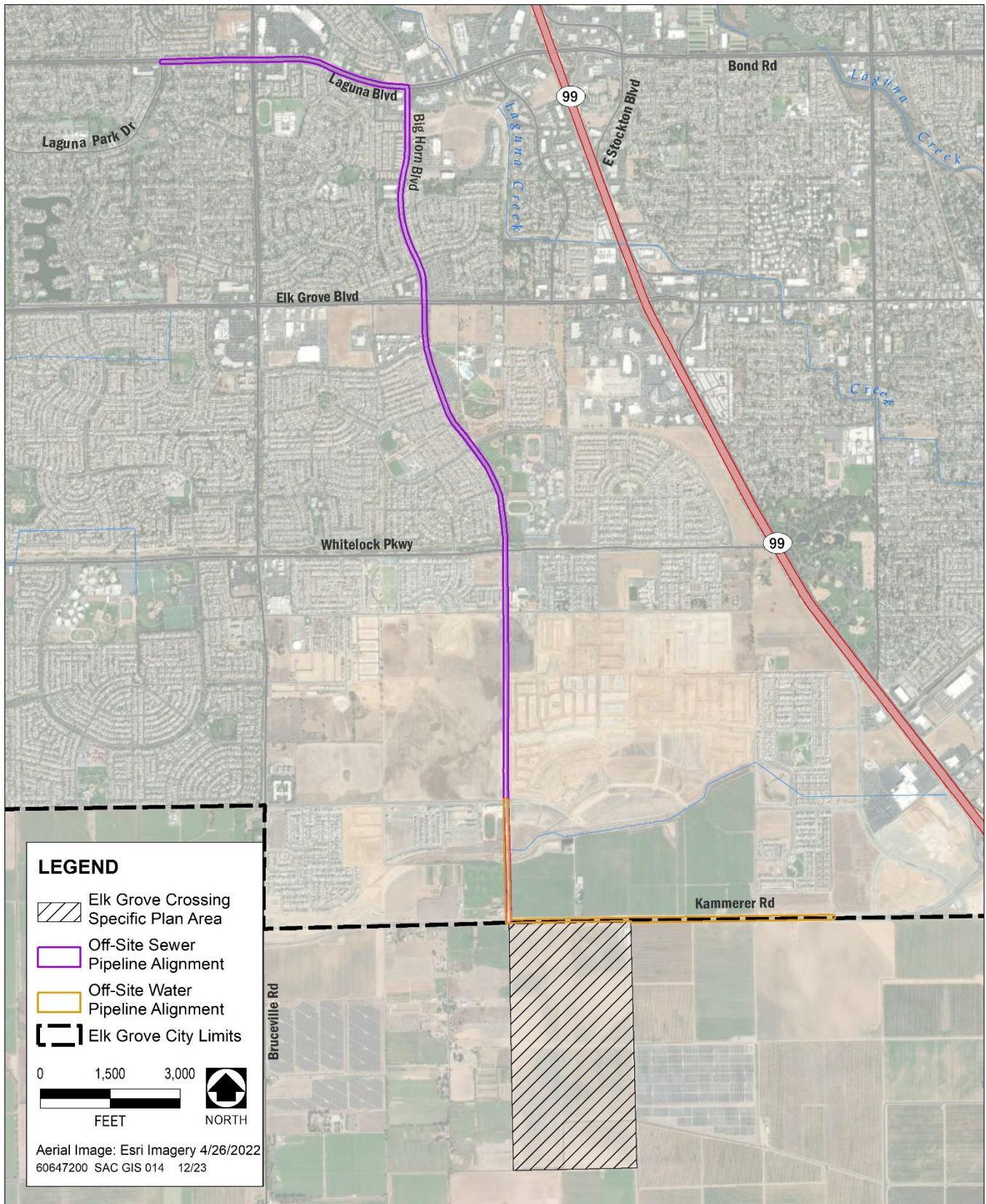
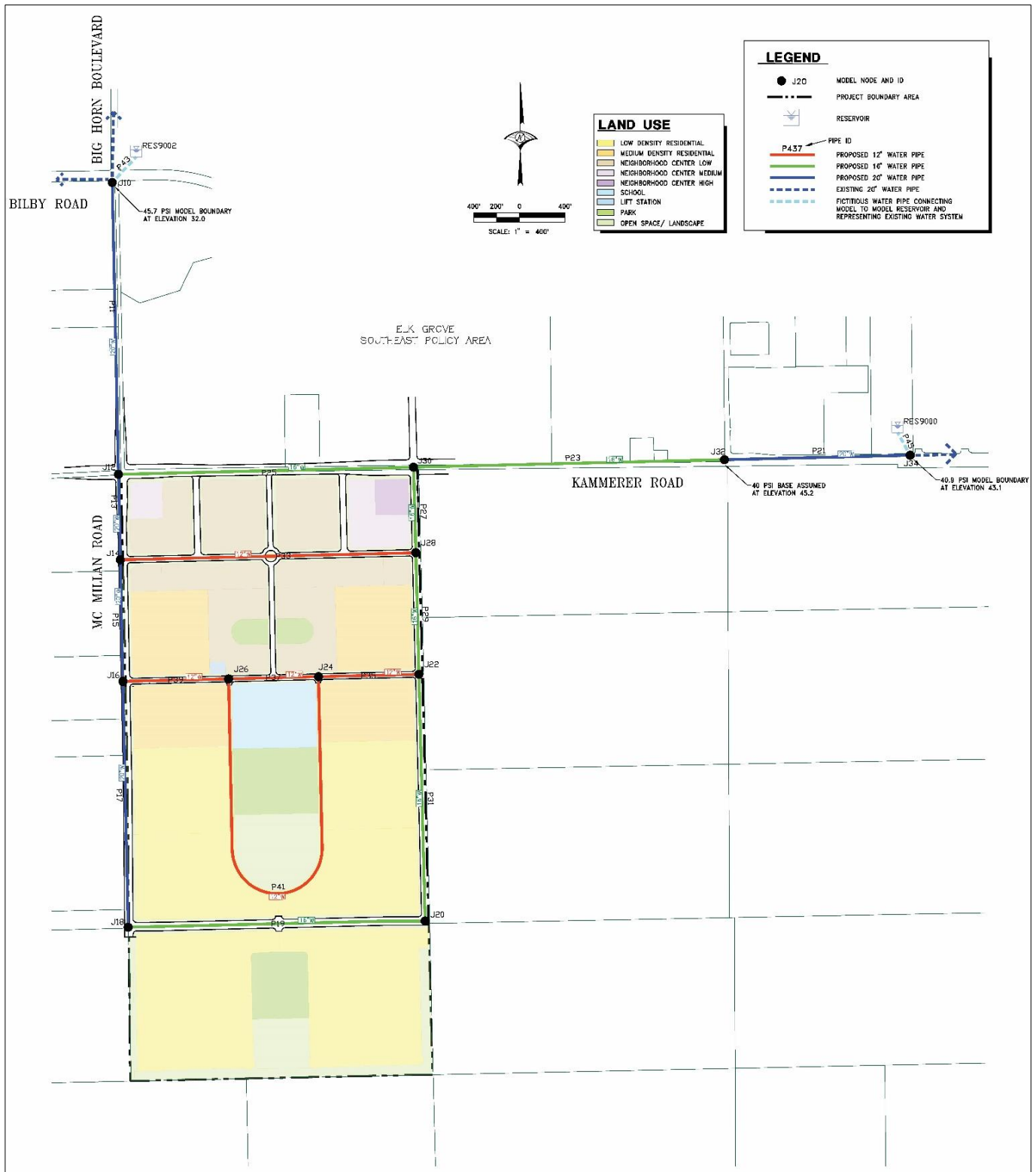


Exhibit 4. Proposed Water System



Source: Wood Rodgers 2024

Exhibit 5. Proposed On-Site Wastewater Collection System

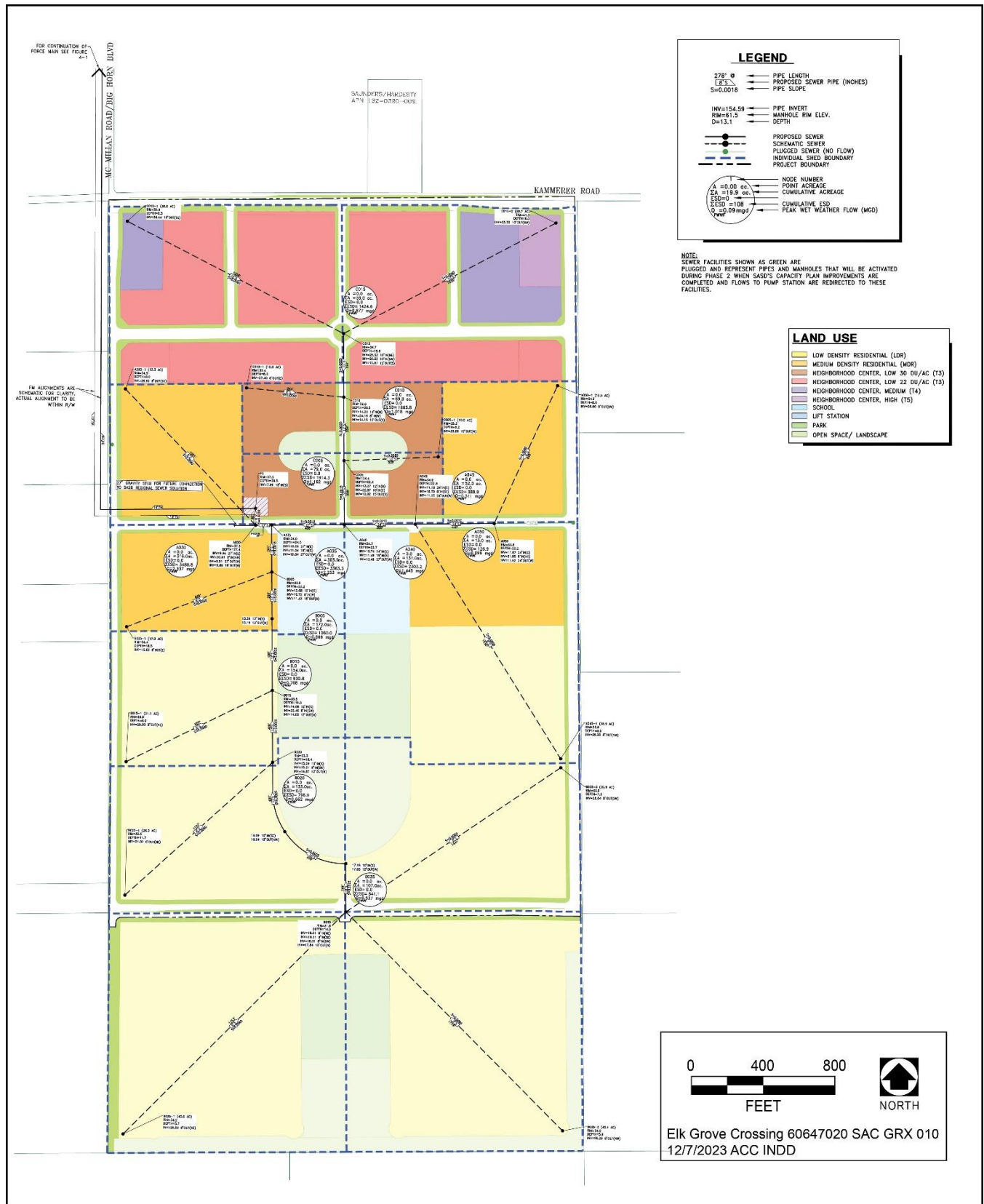


Exhibit 6. Proposed Off-Site Wastewater Conveyance System and On-Site Tie-in

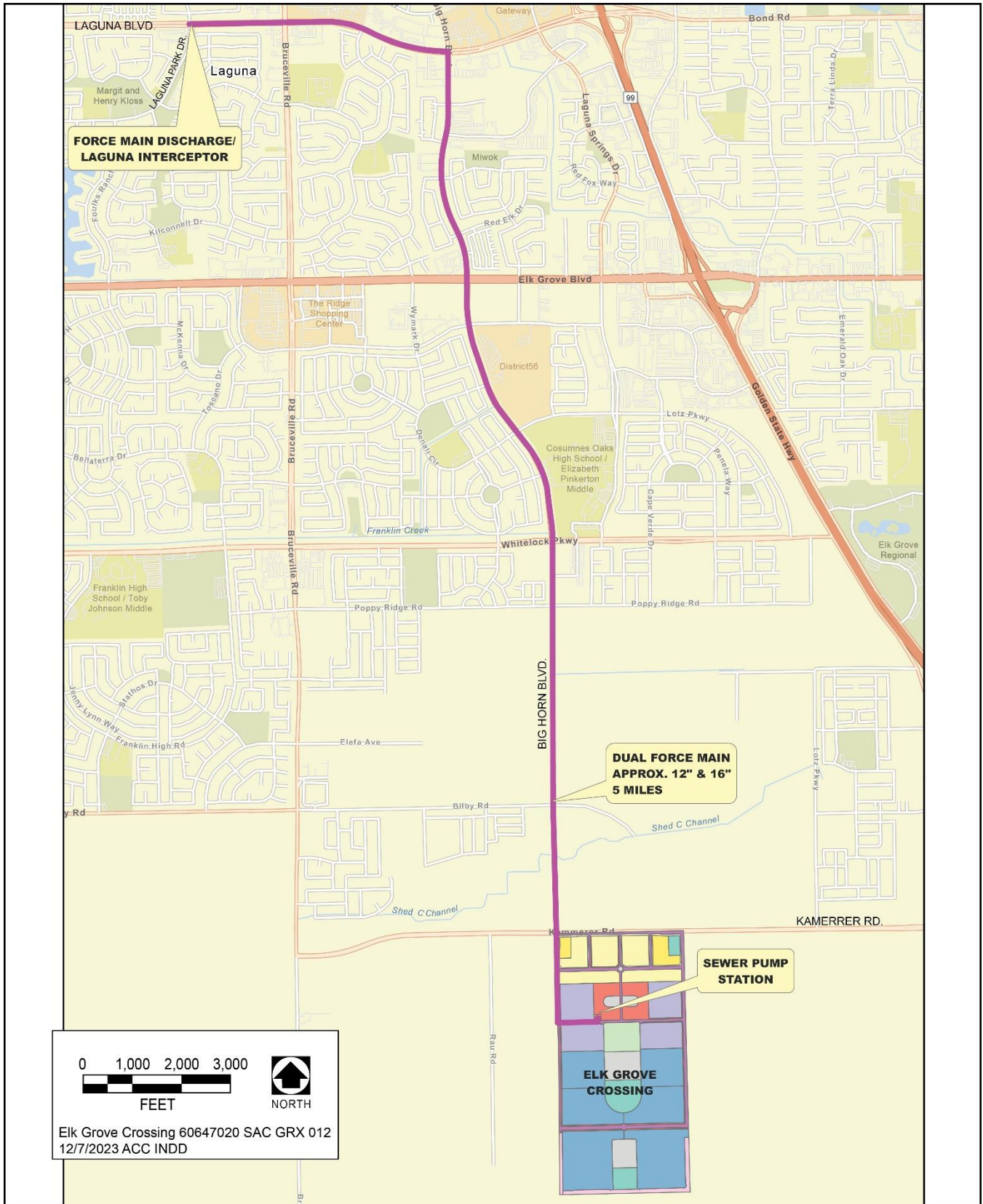


Exhibit 7. Proposed On-Site Vehicular Circulation Network

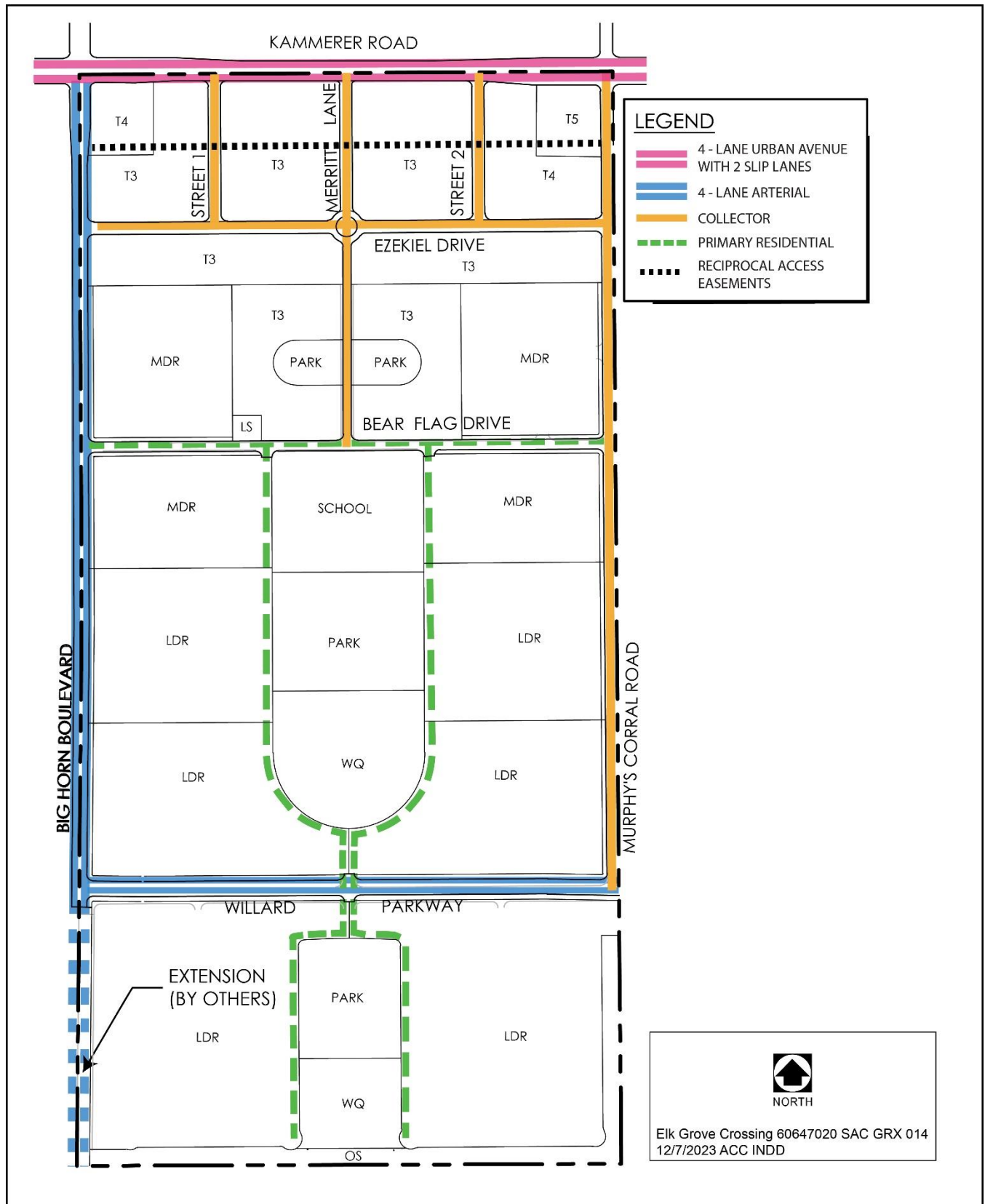


Exhibit 8. Proposed On-Site Bicycle and Pedestrian Circulation Plan

